

## US 17/92 Gateway Corridor Overlay Standards Classification

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## **PART 58 US 17/92 GATEWAY CORRIDOR OVERLAY STANDARDS CLASSIFICATION**

### **Sec 1. Creation.**

In addition to, and supplemental to, all Land Development Code requirements heretofore and hereto after established, there is hereby created an overlay zoning classification known as the “US 17/92 Gateway Corridor Overlay Standards Classification”, hereinafter referred to as the “US 17/92 Corridor.” Property within the US 17/92 Gateway Corridor listed and described within Sec. 3., Designated Gateway Corridor, of this Part, shall be subject to all the provisions herein.

### **Sec. 2. Purpose.**

The purpose of this Part is to insure that the US 17/92 Corridor is developed in a manner which:

- a) Insures that the corridor is developed, and redeveloped, into a high value, functionally strong, aesthetically pleasing and safe gateway to businesses in Seminole County;
- b) Provides uniform building and site design standards to establish high quality private and public development;
- c) Prevents visual pollution caused by unplanned and uncoordinated uses, buildings and structures;
- d) Maximizes traffic circulation and overall mobility using a multi-modal approach from the standpoint of safety, roadway capacity, vehicular and non-vehicular movement and community character;
- e) Enhances property values;
- f) Preserves natural features to the extent practicable; and
- g) Integrates and makes allowances necessary to improve existing uses and buildings.

A further purpose is to provide design guidelines as a resource for the incorporated municipalities to draw upon at their discretion. The standards have been developed with the objective of enabling all land development approving entities adjoining the US 17/92 Corridor to adopt the identical, or similar, standards to ensure a uniform development pattern along this important corridor that traverses the entire County and six of its seven cities.

Another purpose of this Classification is to serve as a base line for coordinating the requirements and processes of the Lake Mary Boulevard Overlay Standards Classification and the State Road 46 Gateway Corridor Overlay Standards Zoning Classification. Further, this classification is the first step in coordinating the several Gateway Corridor classifications into one approach with common processes and definitions while having unique standards for individual corridors.

Redevelopment corridors, such as the US 17/92 Gateway Corridor, have an abundance of small parcels and previously developed parcels that require unique, site-by-site, design solutions. Therefore, a further purpose of this classification is to establish design principles that guide the Planning Manager as she/he interprets this section’s requirements and makes adjustments as authorized, with common sense and flexibility, in order to further the overall purpose of this section. All decision of the Planning Manager may be appealed to the Board of County Commissioners.

The graphic illustrations provided herein are intended to show vision and intended results of this part and serve as an example of a preferred application of the requirements to a typical building or site. The specific requirements may be applied to a unique site in another way.

As an alternative to the strict application of these requirements, the design principles herein provide the basis for adaptations and adjustments that may be suggested by an applicant for site plan approval and, if appropriate, approved by the Planning Manager consistent with the purposes and design principles of this Part.

The design principles have been expressed using the words “shall” and “will” to denote that compromise of the principles is discouraged. Any flexibility of this part should be directed to the design standards themselves, as long as the ideas embodied in the principles are upheld.

### **Sec. 3. The US 17/92 designated gateway corridor.**

The designated gateway corridor subject to the provisions of this Part is US 17/92 including the section of the roadway and adjacent or nearby properties in Seminole County from First Street in Sanford southward to the Orange County/Seminole County line that lie within the unincorporated area of the US 17/92 Community Redevelopment District.

## **Sec. 4. Future gateway corridor study commission.**

The County and the cities within the US 17/92 Corridor coordinate their activities through the US 17/92 Community Redevelopment Agency [CRA]. Coordinating parties include all cities with lands in the district plus the City of Casselberry, which has established a separate and compatible Community Redevelopment Agency for the part of the corridor within its jurisdiction. It is further the intent of this section to coordinate the requirements of the US 17/92 Corridor with the overlay classifications for Lake Mary Boulevard and SR 46. Overlay standards may also be developed for SR 436 and SR 434.

## **Sec. 5. Design Principles**

Design principles establish the fundamental ideas underlying the design standards. Since the standards cannot anticipate every situation, the principles serve as the basis for interpreting or applying the specific standards. Interpretations of the design standards may be necessary to respond to a unique mixed-use opportunity, connection to adjacent uses, natural feature or site situation.

Adherence to these principles will enable private and public investors and owners to fully utilize lands in the US 17/92 Corridor in a way that builds long-term value, creates spatial character, enhances natural resources and improves community aesthetics.

### ***Trees.***

Trees shall be used to frame primary public spaces, streets, parking areas and building entries, to provide shade enhance the pedestrian experience, enhance visual aesthetics and conserve energy.

Building locations shall be determined based on existing on-site canopy trees and vegetation to be preserved when they can serve as site amenities, buffers or other forms of community asset.

### ***Flexibility.***

New investment by private interests will be a significant force in the redevelopment and infill of the US 17/92 Corridor as an aesthetically pleasing, functionally effective and safe location. Flexibility in the application of design standards will be a cooperative effort between the County, municipal governments

and the private investors. As flexibility is sought and provided, the following principles apply:

- 1) Deviance from the strict application of these standards may be provided in order to facilitate the design, construction and maintenance of new infill development and redevelopment in the US 17/92 Corridor when the lot size, the lot configuration, the presence of significant trees or other natural feature exists or other site specific conditions exist that do not lend themselves to the strict application of the design standards. Relief may be provided to the extent necessary to address the specific issue involved.
- 2) Relief from the strict application of these standards may be accompanied by site or building improvements beyond the strict application of this code.
- 3) Relief from the strict application of the standards will be provided consistent with public safety needs and the overall intent of the standards.
- 4) The design standards in the US 17/92 Corridor apply to all sites regardless of size, except sites that are aggregations of multiple individual sites. Such aggregations that include parcels less than one (1) acre in size may be given special consideration by the Planning Manager when these sites are presented as a single unified site plan. The considerations apply to side yard setbacks, shared parking, shared stormwater facilities, access and signage. The purposes and principles of this Part will still be applicable.

### ***Pedestrian-Oriented.***

Pedestrianways should be designed to provide safe, shaded and direct connections. Streets, site circulation and parking areas should be functional, attractive and designed to enhance the pedestrian life of the community.

### ***Balance.***

The balance of physical, economic and social factors related to each site and among several sites is a major element of these standards.

- 1) Development along the US 17/92 Corridor will result in a balance of the site area devoted to the building(s), parking and open space.
- 2) The transportation system should be designed to achieve a balance among vehicular, transit, bicycle and pedestrian mobility.

- 3) A balanced approach to the open spaces on the site including stormwater areas, landscape buffers, trees and conservation lands is important in the design and maintenance of the area.

#### ***Shared Facilities.***

- 1) Stormwater ponds, preserved conservation areas, open space, landscaped buffers and lakefronts should be designed as a holistic system of open space in which each element serves its necessary functional role while creating a functional and aesthetic system in which all elements work together in harmony.
- 2) Individual stormwater areas should be coordinated, if not combined, with adjacent public and private stormwater systems and facilities to reduce the land area and expense of these systems.
- 3) Site areas devoted to on-site parking should be minimized through off-site parking, on-street parking and shared parking, to the maximum extent practical.

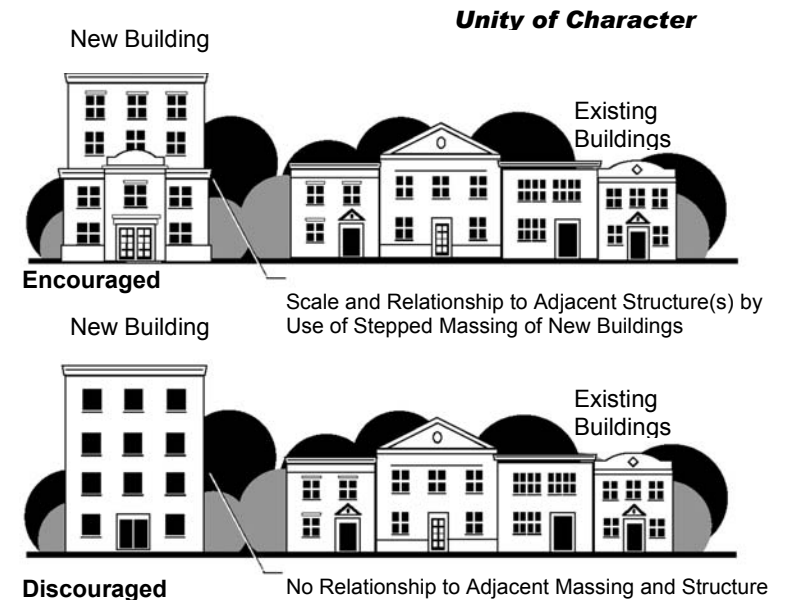
#### ***Connectivity.***

- 1) Connections between and within parcels using a logical pattern of interconnected streets, alleys, cross-access easements, shared driveways, pedestrianways, and bikeways are encouraged.
- 2) The combination, or connection, of several small parcels should be encouraged to let multiple owners act as one unit once the connection of parcels is secured.
- 3) Connections between neighborhoods and US 17/92 businesses can be designed with controlled, calm traffic, pedestrianways and bikeways that protect the neighborhood and enable residents to patronize Corridor businesses through routes other than US 17/92.

#### ***Unity and Context.***

- 1) Buildings, parking areas and open spaces, either new infill development or redevelopment, should have a unity of character internally and with its surroundings. Cluttered forms that have no apparent unity or character are to be avoided.

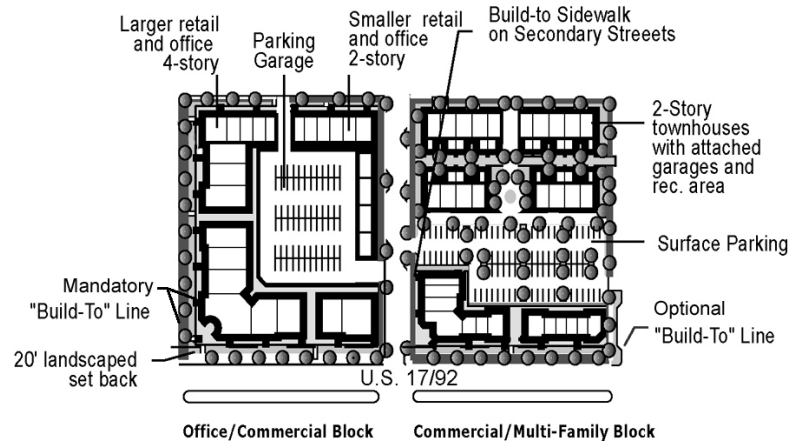
- 2) The building(s) should be designed and located in a manner that enhances the character of the US 17/92 as an attractive, convenient and safe place for businesses and multi-family residences.
- 3) The area devoted to parking should be sufficient to meet the needs of the site occupants while enhancing the aesthetic character of US 17/92.
- 4) Buildings define spaces through the use of uniform setbacks along a “build-to” line along US 17/92.



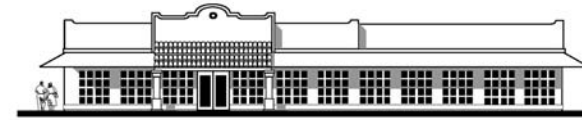
## Mixed-use.

- 1) The mix of compatible retail, commercial, office, multi-family residences and entertainment can create a vibrant and walkable place for businesses and residences.
- 2) Retail and commercial uses on the ground floor of multi-story office and residential buildings help create interesting and vibrant places.
- 3) Buildings should be located to emphasize opportunities for a mix of uses that better use infrastructure with shared or alternative use, especially of buildings, parking areas and stormwater facilities.
- 4) The mixed-use US 17/92 Corridor should be the place where development is promoted and therefore site plans should anticipate site intensification with additional buildings, structured parking and other infill activities that have space and infrastructure which allow for intensification of the site as the market for development grows over time.

## Mixed Use Sites



## Mixed Use Buildings



One-Story Office Building



Mixed-Use, 1st Floor office, 2nd Floor Office or Residential



Mixed-Use, 1st Floor Retail, 2nd floor Office or Residential



Mixed-Use, 1st Floor Office, 2nd Floor Office or Residential



Mixed-Use, 1st Floor Retail, 2nd Floor Office or Residential 3rd Floor Residential

## Sec. 6. Building Locations.

The location of a building on a site is the most important decision in the development process. Buildings define the street and the Corridor through the use of uniform setbacks along the build-to line for each block. The location of a building establishes future opportunities for adaptation, re-use and intensification.

Buildings at street corners of US 17/92 and the primary streets are particularly significant and should be located on the site and designed with architectural features to emphasize their prominent location.

The primary facades of all buildings shall contain the primary entry and face a public street. Frontage on US 17/92 shall receive the highest priority for building placement and façade enhancement.

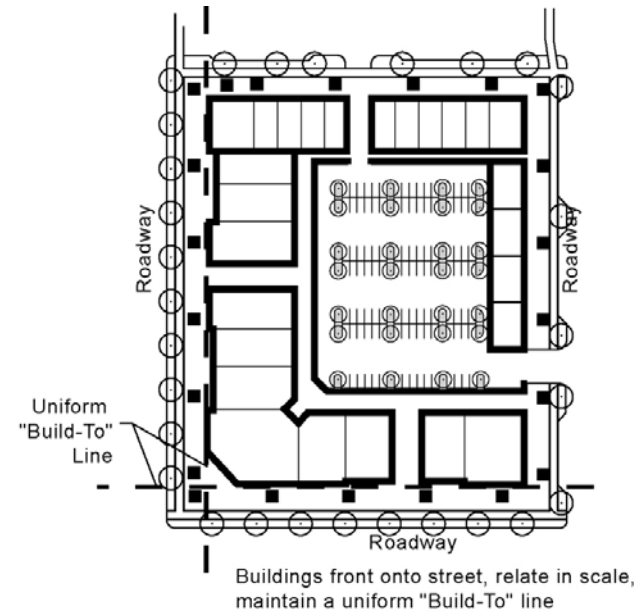
### Building Location – A “Build-To” Line

The purpose of build-to lines is to create a consistent building front along a roadway corridor with all buildings located generally the same distance away from the road. A build-to line is different from a minimum setback. The build-to lines set minimum and maximum setbacks from US 17/92. This creates a “building wall” along the road that defines a space and helps create a sense of place.

Specifically, a build-to-line is a line parallel to the US 17/92 right-of-way line that represents the minimum, and maximum, setback distance from which all or part of a building is constructed from the right-of-way line. Architectural articulations and slight variations in the front building wall plane as it relates to the “maximum” setback distance requirement are usually encouraged to provide for interesting building design.

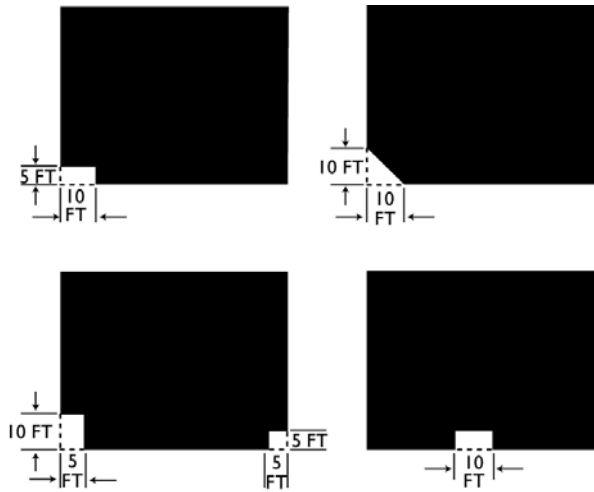
- 1) The build-to line for lots on the corner of US 17/92 and each Primary Street shall be twenty (20') feet on each street. The extent of the zone for this requirement shall be from the intersection to the first full accessway/driveway that serves the site on both US 17/92 and the Primary Street.

### Building Orientation on Corner Lots



Primary Streets in the US 17/92 Corridor, in addition to US 17/92 are designated as:

- 1) 1<sup>st</sup> Street (in Sanford)
  - 2) Airport Blvd.
  - 3) Lake Mary Boulevard
  - 4) SR 427/Five Points
  - 5) SR 434
  - 6) SR 436
- 2) On US 17/92, all other buildings on non-corner lots may be constructed at the build-to line that is 90 feet from the property line on the street right-of-way, or closer to the roadway, but no closer than 20' to the right-of-way line.
  - 3) Articulated building elevations on the street front are encouraged. A portion of the building (less than 20% of the frontage) may be set back from the build-to line up to ten (10') feet to create interest in the building facade. Examples for a building with 50' of frontage are shown below.



#### **Building Location – Design Of Buildings On Corner Lots At Intersections Of US 17/92 and Primary Streets**

- 1) Corner areas shall be designed to emphasize their location. Buildings and structures on corner areas at the intersection of primary streets shall be designed with additional architectural embellishments such as towers, cupolas, or other design features, to emphasize their locations as gateways and transition points within the community.
- 2) Once a design is instituted at a corner, the buildings on the other corners of that intersection shall use a related type of articulation in an effort to achieve design compatibility. The designs should not be identical; however they should generally have the same height and shape.

#### **Building Location - Design for Future Infill.**

- 1) Buildings should be located to enable additional buildings to be located on the site.
- 2) Large surface parking areas shall be visually and functionally segmented into several smaller areas of approximately twenty (20) spaces each. To facilitate infill and redevelopment, land initially devoted to surface parking areas may be exchanged, over time, with replacement parking provided at alternative locations and/or construction of structured parking facilities.

The location of future parking structures and buildings should be anticipated in the original site plan even though actual construction may be years in the future.

- 3) Stormwater ponds may also be replaced at some future time to enable expansion of existing buildings or the addition of additional buildings when the stormwater needs of the site are accommodated in an alternative manner acceptable to the County.

## **Sec. 7. Parking and Impervious Areas.**

### **Parking Areas - Overall Layout**

Buildings and projects, including multiple parcel projects shall be designed to provide safe, convenient, and efficient access for pedestrians and vehicles. Parking shall be designed in a consistent and coordinated manner for the entire site. The parking area shall be designed and integrated with adjacent uses in a manner that enhances the visual appearance of the community.

Parking areas shall be designed to enhance the visibility of and access to the front door.

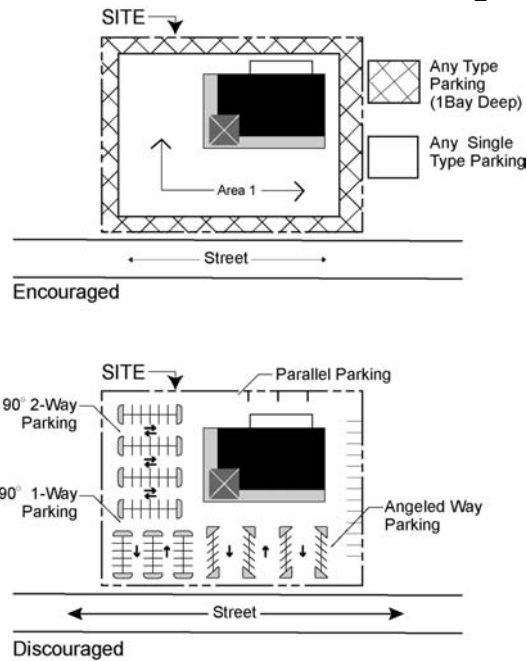
Parking areas are to be treated as enclosed rooms for cars, with enclosure provided by tree planting and/or building wall(s). For small areas (twenty spaces or less), landscaping shall be at the perimeter; for larger areas (more than twenty spaces), landscaping shall be at the perimeter and placed interior to the lot to break the area into parking modules.

### **Parking Areas - Single Type of Angled Parking**

Parking, utilizing the same degree of angle, shall be developed throughout the site to provide efficient and safe traffic and pedestrian circulation. A single bay of parking provided along the perimeter of the site may vary in design in order to maximize the number of spaces provided on-site.

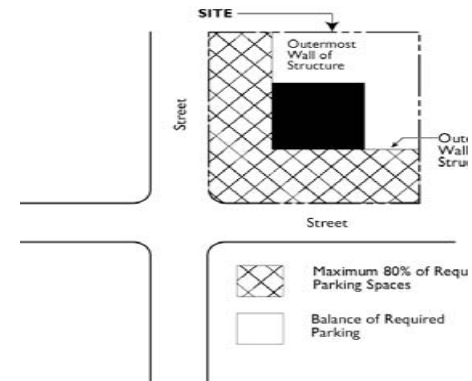
The mixture of one-way and two-way parking aisles, or different degrees of angled parking within any parking area is prohibited, except as noted above, or where individual parking areas are physically separated from one another by a continuous landscape buffer, a minimum five feet in width with limited access.

### Angled Parking



- 2) **Corner lots not at intersections of US 17/92 and Primary Streets.** No more than 80 percent (80%) of the off street parking for the entire non-residential building or project shall be located between any primary façade of the building or project and the abutting streets. No single side shall contain more than 50 percent (50%) of the required parking. (see figure)

### Corner Lots not at Intersections of US 17/92 and Primary Streets

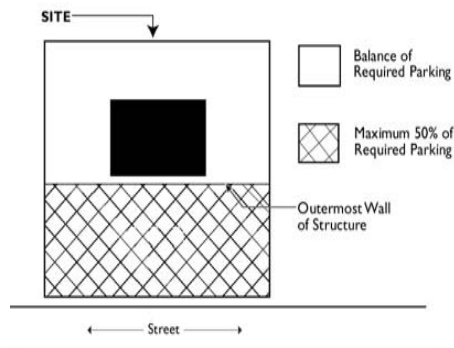


### Parking Area - Locations For Single Building Projects

Single building projects shall be designed to adhere to the following standards:

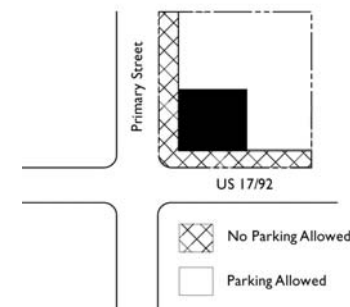
- 1) **Interior lots.** No more than 50 percent of the off-street parking for the entire non-residential building or project shall be located between any primary façade of the principle building and US 17/92. (see figure)

### Interior Lots



- 3) **Corner lots at intersections of US 17/92 and Primary Streets.** No parking shall be located between buildings and the street at intersections of US 17/92 and Primary Streets from the intersection to the first access drive on each street

### Corner Lots at Intersections on US 17/92 and Primary Streets





## **Parking Areas - Minimum Number Of Required Parking Spaces**

Sufficient parking must be provided to serve the particular needs of the building(s). The number of parking spaces required for a specific business, residence or other activity is specified in the Parking Regulations of the Seminole County Land Development Code. The number of spaces required shall be provided, with opportunities for reductions in the number of spaces for mixed use developments using shared parking as described below.

### **Parking Areas – Motorcycle and Bicycle Spaces**

Motorcycle and bicycle spaces may account for up to 2 percent of the required number of spaces.

### **Parking Areas – All Handicapped Spaces on Site**

The required number of handicapped parking spaces shall be provided entirely on-site as required and in accordance with the Americans With Disabilities Act and the operative Standard Building Code.

At least 50% of the required number of parking spaces shall be provided on the site of the buildings or activities for which the parking is required.

### **Parking Areas - Alternative Methods for Providing Required Parking**

Parking can be provided to serve a building or groups of buildings and uses in a variety of ways. The intended use is the destination of a person once they park their car. The Planning Manager can approve the use of any technique, or combination, consistent with public safety considerations and the purpose and principles of these guidelines.

- 1) *On-site Parking.* On-site parking may provide 100% of the parking needs for the site to be used in combination with other acceptable sources of parking. At least 50% of the required number of parking spaces shall be provided on the site of the buildings or activities for which the parking is required. This requirement applies to all situations including the use of shared parking, off-site parking, on-street parking or any combination thereof
- 2) *Shared Parking Single Site.* Where a mix of uses on a single site creates staggered peak periods of parking demand, shared parking calculations may

reflect a reduction in the total amount of required parking. The reduction may not be more than 25% of the total number of spaces required for the sum of all the uses when calculated individually. Retail, commercial, office, residential, and entertainment uses may share parking areas and quantities. Shared use of parking shall be guaranteed by a contract or other legally binding agreement.

- 3) *Shared Parking Multiple Sites.* Where vehicular access is provided between adjoining non-residential sites and the peak operating hours of adjoining uses do not overlap, the uses may share up to 50 percent (50%) of required parking spaces. A pedestrianway plan shall be approved with the site plan that ensures safe and convenient pedestrian access between the parking area and the intended use. Shared use of parking shall be guaranteed by a contract or other legally binding agreement.
- 4) *Off-site Parking.* The number of parking spaces required for a non-residential building(s) or activity may be provided with parking spaces off-site based upon the availability of an off-site and non-contiguous parking facility or parking area. Off-site parking shall be guaranteed by a contract or other legally binding agreement.

The property owner shall provide the following information to support a request for reduction or waiver of otherwise required parking.

- The availability of the off-site parking areas must be guaranteed in perpetuity, by virtue of common ownership with the primary site, recorded easements, or other binding agreements acceptable to the County.
- The off-site parking areas must be located within convenient walking distance [no more than three (300) feet] of the primary site, said distance to be measured along paved public right-of-way from the entrance driveway into the parking area to the entrance driveway into the primary site or another acceptable pedestrianway.
- The off-site parking areas shall have safe pedestrian access provided from the parking area to the intended use with sidewalks or other safe and convenient facilities.
- The off-site parking areas shall meet the same development standards (regarding impervious surface, drainage/retention, buffering, interior landscaping, etc.) as those that apply to the primary site.

- The off-site parking dedicated to use of the primary site will be documented on the site plan, if and when approved by the Planning Manager.
- 5) *On-Street Parking.* Adjacent on-street parking may be counted towards a non-residential building or uses parking requirement. The amount of on-street parking can be maximized up to 50% of the required parking spaces as long as the on-street parking is on the block face of the intended use and no streets need to be crossed to walk from a designated on-street parking space to the intended use. On-street parking for non-residential uses shall not be allowed on residential streets or on US 17/92.

### Parking Areas – Screened Parking on Corner Lots at Intersections of US 17/92 and Primary Streets

The intent of these design standards is to screen parking areas from the view of US 17/92 travelers and to create a minimum building frontage of 50% of the site's US 17/92 street frontage on corner lots on Primary Streets. The following standards shall apply:

- 1) Where a building frontage occupies 50% or more of the frontage, the parking frontage shall be screened using one of the following alternatives.

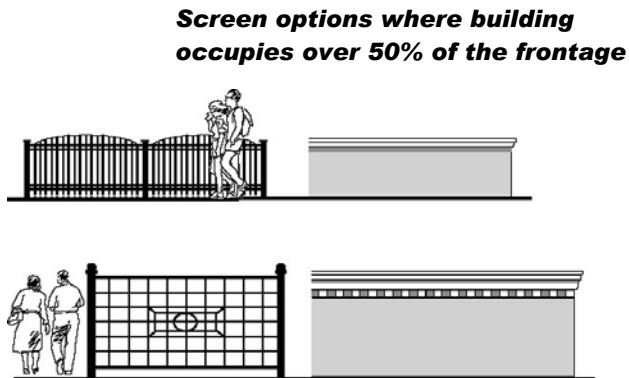
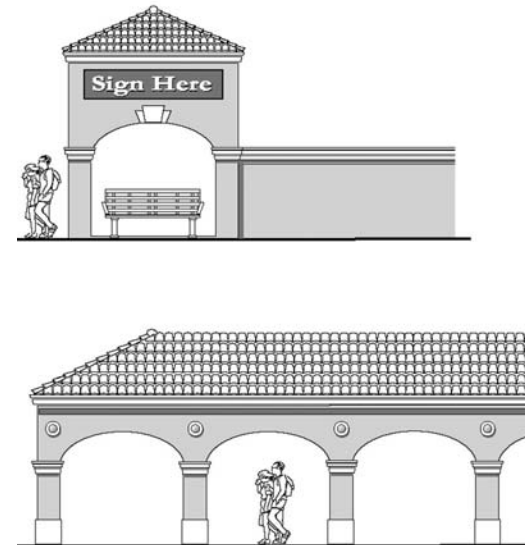


Figure 1

- 2) Where the building frontage occupies less than 50% of the street frontage the parking area shall be screened using one of the alternatives in the following figure to the extent that this parking screen plus the building length equals at least 50% of the site's frontage on the street. The balance of the parking frontage and screening may use alternatives from Figure 1.

### Screen options where building occupies less than 50% of the frontage



### Parking Areas – Interconnected

Parking areas adjacent to one another whether owned and managed by the same entity or different entities, shall connect the parking areas and pedestrianways to permit safe and efficient travel by vehicles and pedestrians.

### Parking Areas - Access

Parking areas may be accessed from side streets and/or adjacent properties as long as these access drives do not adversely affect adjacent or nearby residences. Access from US 17/92 shall be minimized in order to re-create acceptable spacing between driveways consistent with County and FDOT standards.

### Parking Areas - Side and Rear

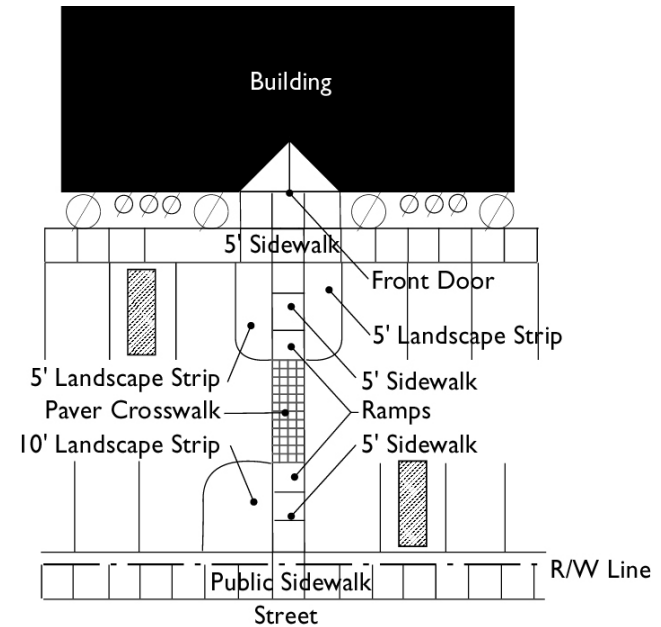
On-site parking for employees and customers may be located in safe, well-lighted parking areas behind or on the side of the building. Active/Passive Buffer requirements apply to protect adjacent residences from obtrusive activities that may intrude from the parking area into an adjacent residential area.

### Pedestrianways – On-Site System Design

The pedestrian access path from the public sidewalk to the main building entrance and elsewhere on the site as necessary to facilitate safe and convenient walking, shall meet the following minimum design requirements:

- Five foot (5') wide paved surface (concrete, bricks, or pavers);
- Pavement markings and a change of paving materials at all crossings of vehicular use areas;
- Minimum ten-foot (10') wide landscape strip along one side (or five foot (5') on each side) of the connecting pathway. Landscaping provided for pedestrian pathway may be counted toward the landscape requirements for parking area and building base;
- Pedestrian lighting; and
- Ramps as required for handicapped persons.

### Pedestrian Connection Standards



### Driveways – Shared

Driveways on US 17/92 and Primary Streets in the district shall be combined as shared accessways with adjacent properties to reduce the number of driveways on US 17/92 and to create driveway spacing in conformance with standards for driveway spacing on US 17/92. The creation of shared driveways shall be completed in conjunction with new infill development or redevelopment in the District.

### Driveways – Width

The maximum driveway width permitted by the Seminole County Land Development Code standards is forty feet (40'). Existing driveways that exceed this width will be reduced in width to meet this standard, or a lesser width when appropriate, in conjunction with new infill development or redevelopment in the US 17/92 Corridor.

## Loading and Service Areas

When required, loading docks, solid waste facilities, recycling facilities and other service areas shall:

- Be placed to the rear or side of a building in an unobtrusive locations that is not visible from US 17/92,
- Screen and landscape such areas to prevent direct views of loading areas and their driveways from adjacent properties or from the public right-of-way,
- Screen and landscape such areas to prevent spillover glare, noise, or exhaust fumes,
- Screen and buffer such areas using the Active/Passive Buffer standards with walls and landscaping to obscure the contents and facilities. Recesses in the building, or depressed access ramps may be used.

## Parking Areas – Bicycle Circulation and Parking

- 1) Bicycle accessways to buildings shall be identified on Site Plan.
- 2) Bicycle parking shall be provided at retail, commercial, office, multi-family residential and entertainment activities.
- 3) Bicycle parking space shall be provided at a rate no less than 2% of the total required parking spaces.
- 4) Bicycle racks or other storage facilities shall be provided as follows:
  - Facilities shall be designated to allow each bicycle to be secured against theft.
  - Facilities shall be installed so as to resist removal.
  - Facilities shall be installed so as to resist damage by rust, corrosion, or vandalism.

## Parking Areas – Reduce Scale and Parking Lots

Land devoted to surface parking lots may be reduced to accommodate infill development, redevelopment and/or construction of structured parking facilities at the discretion of the Planning Manager when the purpose of the requirements is sustained and the functional needs of the use are met such that adjacent parking areas and street frontages without designated on-street parking spaces will not be adversely affected.

## Parking Areas – Structured Parking

Architecturally, structured parking facilities are to stress compatibility in appearance, size, scale, and bulk of the structure with its surroundings in the design. The architectural design proposal should address, but not be limited to the following:

- Minimizing blank concrete facades;
- Use of textured concrete, planter, and trellises;
- Achieve architectural unity with adjacent buildings; maintain a common architectural character with the principal building;
- Incorporate at a minimum an equal portion of vertical and horizontal architectural elements;
- Replicate the regular window pattern and other architectural elements of adjacent buildings;
- Incorporate art into the structure's façade.

### ***Architectural Treatments for Structured Parking***



## Sec. 8. Open Space, Buffers, Stormwater Ponds, Lakefronts and Landscaping in Parking Areas

### Open Space

The US 17/92 is a mixed-use development corridor. As such, the emphasis for design and development is based on increasing intensity, connections and the mix of uses along the Corridor. Open space, or green space, is expected to be provided through new landscaping within the US 17/92 right-of way itself, within the parking areas, within the building setbacks and within the active/passive buffers along the back edge of the Corridor.

No special open space requirements apply to the US 17/92 Gateway Corridor District other than those described elsewhere in the Seminole County requirements.

## Residential Buffers

Buffers between the mix of active uses encouraged in the US 17/92 Gateway Corridor District and any adjacent residential neighborhoods are critically important. The Active/Passive Buffer requirements shall be strictly enforced with waivers provided by the Planning Manager only with the concurrence of the Board of County Commissioners through their Consent or Regular Agenda.

The requirements of the Seminole County Land Development Code **Sec. 30.1232. Active/passive buffer setback design standards** shall apply in the US 17/92 Gateway Corridor. In addition, buildings in the US 17/92 Gateway Corridor shall be required to increase their setback from residential properties by 25 feet for each story above 2 stories with a maximum required setback of 350 feet. The height of the required wall in the buffer shall be measured from the finished surface of the parking lot which it is intended to screen.

## Stormwater Ponds

Stormwater ponds will continue to be constructed within the US 17/92 Gateway Corridor District as redevelopment and infill development occurs. In all cases, stormwater ponds which are visible from a public street shall be developed as aesthetic features for the site and the Corridor and shall:

- Be landscaped with canopy trees spaced every 100' around the edge of each pond with sod or ground cover,
- Be designed with slopes that are safe without fencing, and
- Be designed for ease of maintenance.

Fountains and lighting are encouraged to enhance the appearance of the ponds. Off-site stormwater treatment or disposal may be utilized. The off-site facilities must be acceptable to the County. Off-site stormwater facilities shall be guaranteed by a contract or legally binding agreement acceptable to the County. The reuse of stormwater for irrigation of the site is encouraged.

## Lakefronts

Lakes, lakefronts and edges in the US 17/92 Corridor are important visual assets in two ways. First, lakes visible from US 17/92 serve as visual amenity with natural qualities that soften and enhance the developed character of the Corridor. Second, lakes in the Corridor usually have residences on the far side, away from the US 17/92 development. It is important to anticipate the situation where lakefront

residents view, across the lake, the back of commercial and the non-residential buildings and activities.

For these two reasons, standards exist for improvements to protect and enhance the visual quality of lakeshores in the Corridor. Improvements shall include canopy trees appropriately spaced with ground cover or sod and benches or other passive recreation features that enable pedestrian views of the water.

Development within the US 17/92 Corridor that is on a water body with residential areas elsewhere on the shore of the same water body may not allow launching of motorized watercraft and shall provide:

- Canopy trees, 4" in size, along the shoreline spaced every 100',
- Screening of the areas in the back of any commercial, office, retail, multi-family residential or entertainment buildings or other active uses that are unsightly, such as loading areas, dumpsters, parking areas, [no outside storage is permitted], and equipment on the ground, on roofs or other areas,
- A lakeshore walkway along the entire length of the developer/owners frontage, and
- Lighting that creates a safe area without creating glare across the water.

## Wetlands and Conservation Areas

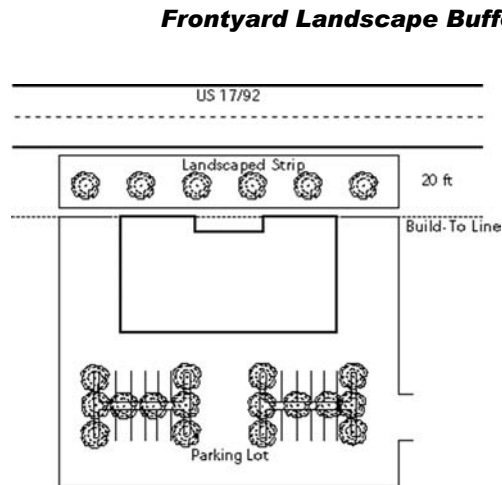
Wetlands and conservation areas designated for preservation will be designed as part of a holistic system in concert with buffers, stormwater ponds, lakefronts and landscaping.

## Landscaping - Parking Areas

These provisions are intended to achieve enhanced visual quality of parking areas, encourage the creation and protection of open space, avoid expansive development of impervious surfaces, and ensure that landscaping is an integral part of site development and the Corridor development.

- 1) *Front yard Landscaped Buffer Strips*- A front yard landscaped buffer strip shall be provided separating all parking areas, vehicular circulation facilities, or similar improvements from the right-of-way line of US 17/92 and any public street. The landscaped buffer strip shall be continuous except for required vehicular access points, pedestrianways and connections to adjacent properties. Signs may be integrated into the landscaping within the landscape buffer strip. Buffer strips shall not prevent connections among adjacent parking areas.

- The landscaped buffer strip along the US 17/92 frontage shall be twenty feet (20') in width.



- The landscape buffer strip along other streets in the District shall be ten feet (10').
- Landscape buffer strips shall be protected from vehicles in the parking area with curbs or wheel stops for those parking spaces adjacent to the buffer strip.
- Wheel stops shall not be used in the interior portion of the parking area.
- The landscape buffer strip on side streets and US 17/92 may be combined with approved on-site, wet or dry-bottom stormwater retention areas since these areas shall be designed, or redesigned, as visual amenities without fences, with sod and with Designated Shade Trees.
- The landscape buffer strip shall consist of sod or other acceptable ground cover and one Designated Canopy Tree for every fifty feet (50') of street frontage along US 17/92 and one hundred feet (100') on other streets in the District.

- 2) **Interior Parking Area Landscape Requirements-** The interior of all parking lots shall be landscaped to provide shade and visual relief from large expanses of impervious surfaces. This shall be achieved by installing protected planting islands within the parking area. Protected planting islands may be "terminal islands" at the end of parking bays or "interior islands" within the parking area.

- Landscaped areas, including the landscape buffer strip, terminal islands and interior islands, shall comprise a minimum total of 10 percent of the total area of parking lots.
- At least one landscaped island, terminal or interior with one Designated Canopy Tree, shall be provided such that no row of parking shall exceed ten contiguous spaces. Understory trees may be allowed as determined by the Planning Manager.
- Landscaped Islands shall be located either to separate parking spaces from driveways and other vehicular travel lanes, or to break up large numbers of parking spaces in a single row of spaces.
- Landscaped Islands shall be at least eight feet (8') wide exclusive of curbs and pavement and shall extend the entire length of the adjacent parking space

### Landscaping - Street Trees on US 17/92

- 1) **Spacing and Placement** - Street trees shall be planted within the front yard, and along any yard or setback area facing US 17/92, along all lots.
  - At a minimum average spacing of fifty feet (50') on center, with the exact spacing to be evaluated on a site-specific basis; trees may be spaced as far apart as every one hundred (100') feet or part thereof where circumstances allow.
  - Located as far away as possible from overhead power lines; this may include locating the street trees within the adjacent landscape islands in the parking lot.
  - All street trees along US 17/92 shall be Live Oaks or Laurel Oaks and shall have a clear trunk of six (6) feet and a minimum caliper of six (6) inches at the time of planting.
  - The Planning Manager may permit street trees to be planted in the public right-of-way through the site plan review process.
- 2) **Materials** - All trees, plants and any other associated landscape or outdoor material shall be of Florida No. 1 or better, as classified by the Florida Department of Agriculture and Consumer Services using the County's plant list.
- 3) **Replacement and Maintenance** - Street trees may be limbed up from the bottom, not "lollipopped" on the sides and tops, to improve visibility of signs and window displays with the District from US 17/92

## Landscaping – Buildings on Corner Lots

Landscaping on sites at the intersection of US 17/92 and any other Primary Street shall be designed to re-enforce the character and unity of the entire intersection consistent with the intent of the building architecture described above.

## Sec. 9. Building design

### Building Design - Base and Top Treatment –

Building design should contain the following:

- 1) A recognizable “base” consisting of (but not limited to): (a) thicker walls, (b) richly textured materials (e.g. stucco, tile or masonry treatments), (c) special materials such as ceramic tile, granite and marble, or (d) contrasting colored materials, mullions, and/or panels.
- 2) A recognizable “top” consisting of (but not limited to): (a) cornice treatments, (b) roof overhangs with brackets, (c) stepped parapets, (d) richly textured materials (e.g. stucco, tile or masonry treatments), and/or (e) differently colored materials; colored “stripes” do not fulfill this requirement. Sample treatments include, but are not limited to those shown below.

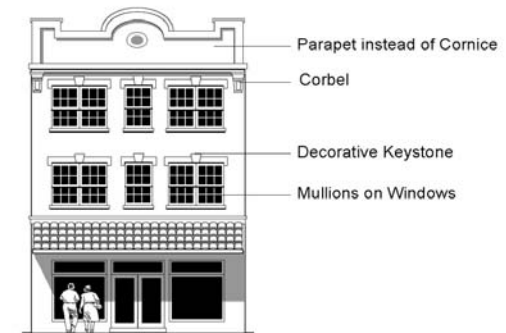
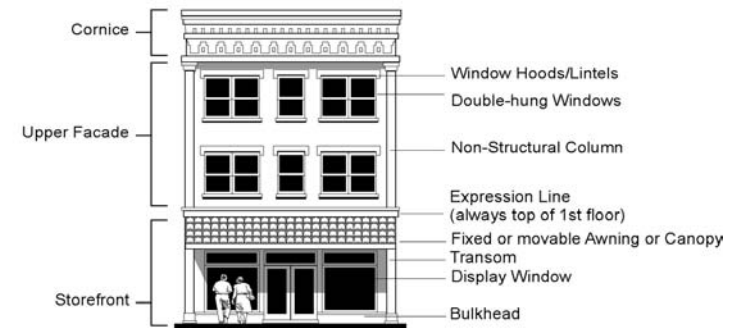
### Building Design - Entries Highlighted

*Highlight Building Entries.* Building massing, architectural details, entry details or changes in materials should be used to highlight the location of building entries. For example, greater height can be used to accentuate entries in the form of tower elements, tall voids, or a central mass at an entry plaza (*see figure*).

### Clearly Defined Building Entries

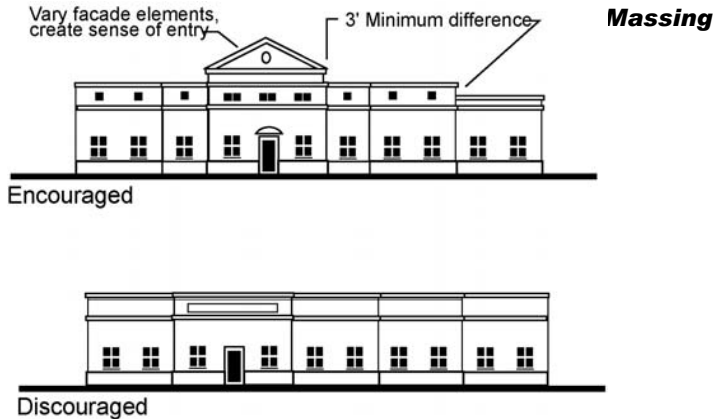


### Base and Top Treatment



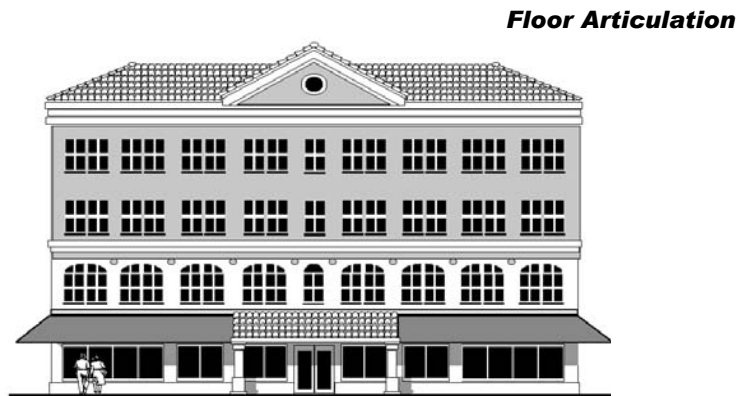
## Building Design - Massing

*Vary Massing of Large Buildings.* A single, dominant building mass shall be avoided. Substantial variations in massing shall include changes in height and horizontal plane. False fronts or parapets create an insubstantial appearance and shall be avoided. Stepping the building can also reduce the apparent scale of the building



## Building Design - Floor Articulation

In buildings in excess of 3 stories, the first floor or first two floors should have distinctive material changes, textural changes, changes in ornamentation and/or colors that are different from the upper floors. Often there is a cap or bottom detail. A change of plane of at least 6 inches and cap or support detail will also achieve this result. The *Floor Articulation* figure illustrates acceptable forms of floor articulation.

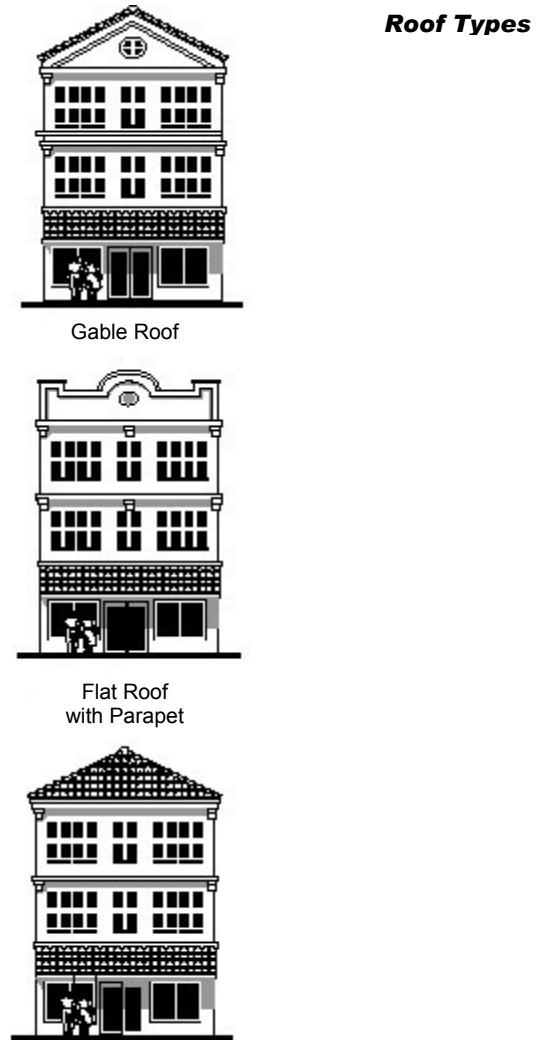


## Building Design – Elevations

Although the primary aesthetic emphasis shall be on the main façade(s), all building elevations should receive architectural treatment.

## Building Design - Roof Types

Roof types should be front- or side-gable, hipped, or flat with parapet or a combination thereof. The principal roof pitch should be a minimum of 3:12. Mansard roofs are prohibited.





### Building Design - Roof Overhangs

Unless specifically designed otherwise, roof overhangs should wrap around all four sides of the building so that there is visual continuity around the entire building unless site specific conditions warrant otherwise.

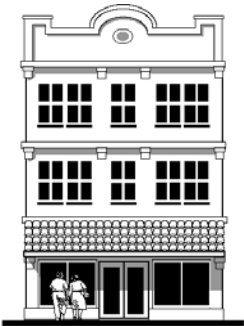
### Building Design - Roof Materials

Roofing material should be constructed of durable high quality material in order to enhance the appearance and attractiveness of the community. Permitted roof materials for pitched roofs should be compatible with the character and design of adjacent buildings.

### Building Design - Storefront Transparency

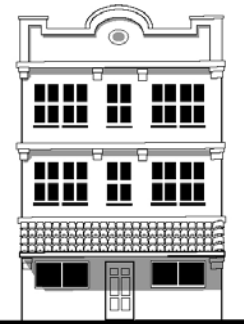
Ground floor windows, in all non-residential buildings within the Corridor, should be transparent glass and framed by the surrounding wall. The transparent area should encompass a minimum of 65% of the total ground-level façade area. Transparency shall be measured from a point not higher than 18 inches above grade.

#### ***Storefront Transparency***



Appropriate storefront with 85% first floor window transparency invites passerby interest and increased employee safety.

**Encouraged**



Incongruous storefront with 15% first floor window transparency and solid door is not a part of the street activity.

**Discouraged**

### Building Design - Roof Mounted Equipment Screening

All equipment on the roof of each building in the US 17/92 Corridor shall be screened so that it is not visible by a person standing anywhere on the site or on an adjacent public street.

### Building Design - Height

The maximum height of buildings within the US 17/92 Gateway Corridor shall be either 2, 4 or 10 stories depending on the location of the building on the site and along the corridor as follows:

- 1) For buildings that are within 750' of the intersection of US 17/92 and a Primary Street and more than 200' from single family residential areas (existing, zoned or planned), the buildings may be a maximum of 10 stories, or 100' in height.
- 2) For buildings that front on US 17/92 and conform to the suggested twenty foot (20') "build-to" line, and have a separation of more than 100' from the building to any adjacent single family residentially zoned property, buildings may be a maximum of 4 stories, or 50' in height.
- 3) For all other buildings the maximum building height will be 35', unless a lower limit is designated in the underlying zoning designation for the property, or a higher limit is approved by the Planning Manager based on the purpose of this section and the requirements of the active/passive buffer standards and site specific conditions.

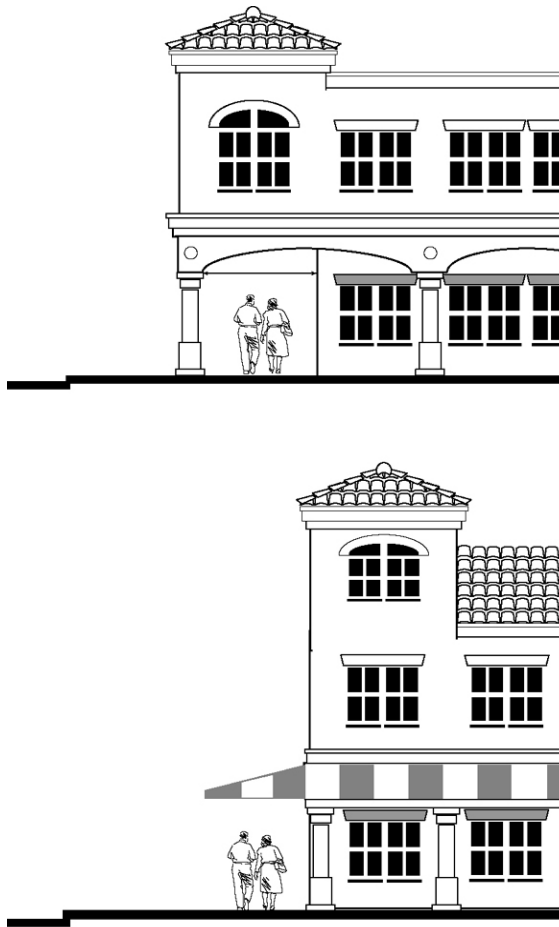
### Building Design – Covered Walkways

Buildings along the US 17/92 Corridor shall provide covered walkways along the side of the building facing US 17/92. Covered walkways shall conform to the following provisions (*see figure*):

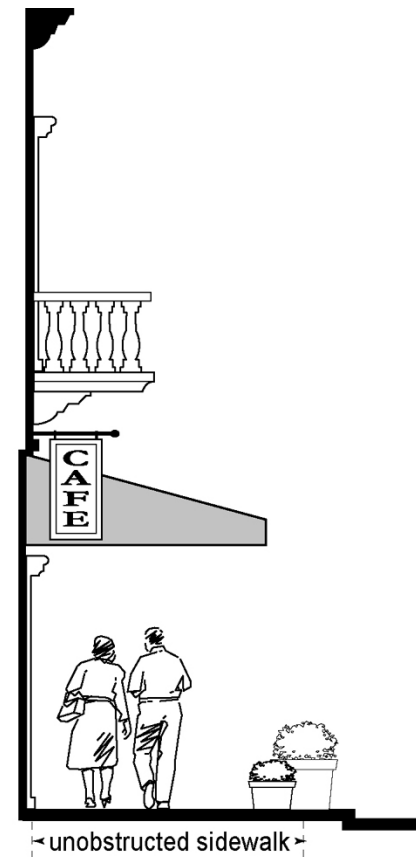
- 1) All covered walkways shall have a clear width for an unobstructed walkway of at least six (6) feet for pedestrian movement.
- 2) Walkways may be covered by part of the building, a "shed" roof, awning or other weather protective surface.
- 3) All covered walks, shall have an unobstructive clear height of nine (9') feet.

- 4) The details of the covered walkway shall be consistent with the architectural style of the main building.
- 5) All covered walkways shall be lighted to illumination levels equal to those on surrounding sidewalks.
- 6) Freestanding buildings with less than 5,000 gross square feet of area are not subject to this requirement; but shall have a covered entryway that provides weather protection at entryways in a manner consistent with the architectural style of the building.

#### ***Covered Walkways***



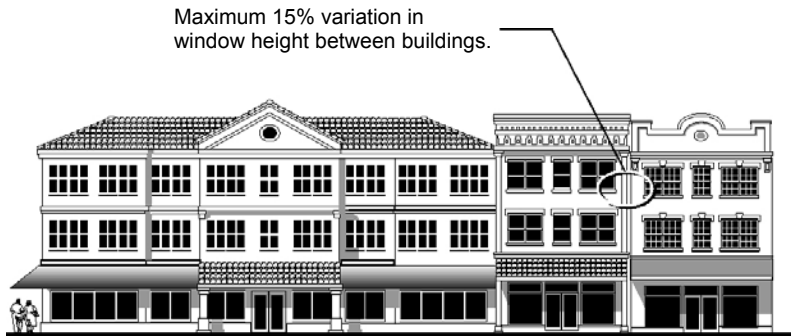
#### ***Building Projections***



## Building Design - Windows and Doors Details

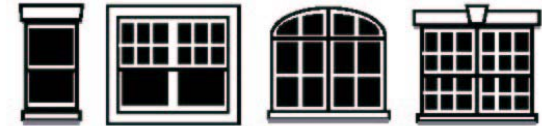
- 1) The windows and doors should be in vertical and horizontal alignment and have a harmonious aesthetic appeal. (see *Window-Door Alignment* figure.)
- 2) Windows should be vertical or square in proportion. (see *Window Treatment* figure.)
- 3) Windows should be inset a minimum of 2 inches from the exterior wall surface. (see *Recessed Window* figure.)
- 4) Mirrored glass is discouraged.
- 5) Glass curtain walls are discouraged.
- 6) Clear glass should be used for storefront windows and doors.
- 7) Exterior mounted security gates or solid roll down metal windows should not be permitted.
- 8) Link or grill type security devices should be permitted only if installed from the inside, within the window or doorframes.
- 9) Other types of security devices fastened to the exterior walls should not be permitted.

### Window-Door Alignment

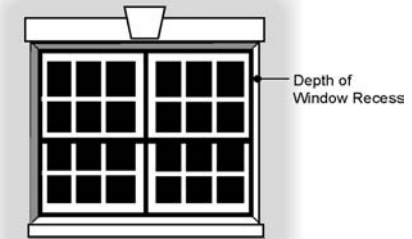


Some variation in window design is encouraged providing “natural”, built over time, appearance for an area. However heights of windows should not vary more than 15 percent from building to building providing necessary continuity.

### Window Treatment



### Recessed Windows



## Sec. 10. Building and Site Design Standards for Special Uses and Activities.

### Automobile service stations [in addition to Sec. 30.1352. Automobile Service Stations]

- 1) *Clearance Required.* Gasoline pumps, canopies and all related appurtenances shall be located at least twenty (20) feet from any street right-of-way line and at least ten (10) feet from any property line. No gasoline pumps shall be located within one hundred (100) feet of any property which is residentially zoned.
- 2) *Buffers and Setbacks When Adjacent to Residential Uses.* When an automobile service station is adjacent to residential uses, single family or multi-family, the use shall be considered active when applying the Active/Passive Buffer Standards.
- 3) *Buffers and Setbacks When Adjacent to Non-residential Uses.* When an automobile service station is adjacent to non-residential uses, walls [not fences], landscaping and setbacks will be established in the Site Plan to screen headlights and noise from adjacent uses. Connections between uses are still encouraged.
- 4) *Buffers and Setbacks When Adjacent to an Arterial Roadway.* When an automobile service station is adjacent to an arterial roadway, such as US 17/92 wall or hedge shall be provided at the right-of-way line on the arterial roadway frontage with adequate openings for vehicular and pedestrian access to provide safe sight distance lines. The finished grade of the paved surfaces in the vicinity of the wall shall be at the same or lower grade as the foundation of the wall.
- 5) *Lighting* – In addition to the requirements of **Sec. 30.1233. Miscellaneous design standards.** (b) Exterior Lighting, the following standards shall apply to automobile service stations:
  - Parking, service area and driveway lights shall not exceed 18' in height.
  - Lighting under canopies or awnings shall be recessed flush with the surface of the canopy or awning with the lens flush with the surface.
  - The light source/light bulbs for any exterior lighting, whether for signs, entrance doors, service or drop-off areas, shall be shielded and not directly visible from public view, regardless of the mounting height.
  - Lighting at ATM's shall comply with Florida Statutes.
  - All lighting shall be designed as recommended by the Illuminating Engineering Society (IES).

- 6) Architectural features of the building(s) when part of a larger development, such as an out-parcel, should have a compatible architectural style.
- 7) All buildings should have a finished roof (hip, gable, or parapet) on all elevations of the buildings.
- 8) The building(s) will be located in a manner that re-enforces the creation of a definitive street edge, whenever possible.
- 9) The buildings will have a pedestrian connection between the building(s) and the sidewalk on the roadway.
- 10) Driveways on US 17/92 shall be minimized through the use of shared driveways, cross-access easements that provide access through other properties and compliance with driveway width standards.
- 11) Any use of the 17/92 right-of-way for uses other than approved vehicular and pedestrian access is expressly prohibited.

### Automobile, boat, recreation vehicle and related equipment sales and display lots

- 1) *Buffers and Setbacks When Adjacent to Residential Uses.* When a sales or display lot is adjacent to residential uses, single family or multi-family, the use shall be considered active when applying the Active/Passive Standards.
- 2) *Buffers and Setbacks When Adjacent to Non-residential Uses.* When an automobile sales lot is adjacent to non-residential uses, walls [not fences], landscaping and setbacks may be established in the Site Plan to screen headlights and noise from adjacent uses. Connections between uses are still encouraged.
- 3) *Lighting* – In addition to the requirements of **Sec. 30.1233. Miscellaneous design standards.** (b) Exterior Lighting, the following standards shall apply to automobile sales lots:
  - Parking, vehicle and equipment storage and display area, service area and driveway lights shall not exceed 18' in height.
  - Lighting under canopies or awnings shall be recessed flush with the surface of the canopy or awning.

- The light source/light bulbs for any exterior lighting, whether for signs, entrance doors, service or drop-off areas, shall be shielded and not directly visible from public view, regardless of the mounting height.
  - All lighting shall be designed as recommended by the Illuminating Engineering Society (IES).
- 4) Architectural Features of the building(s) when part of a larger development, such as an out-parcel, should have a compatible architectural style.
  - 5) All buildings should have a finished roof (hip, gable, or parapet) on all elevations of the buildings.
  - 6) The building(s) will be located in a manner that reinforces the creation of a definitive street edge, whenever possible.
  - 7) The buildings will have a pedestrian connection between the building(s) and the sidewalk on the arterial roadway.
  - 8) Driveways on US 17/92 shall be minimized through the use of shared driveways, cross-access easements that provide access through other properties and compliance with driveway width standards.
  - 9) Any use of the public right-of-way, such as the storage and display of vehicles, equipment, signs, flags or any other items on a permanent or temporary basis, is expressly prohibited and subject to code enforcement processes in the Seminole County Code.

#### **Retail and Food Businesses with Drive Thrus**

- a) Drive-thru windows shall not be placed between the right-of-way of US 17/92 and the associated building unless:
  - An eight (8) foot wide landscaped buffer with two (2) canopy trees (4" caliper) and plantings or walls of approximately forty (40") inches in height for a length sufficient to screen the entire drive-thru cueing and stacking area is installed and maintained.
  - A permanent porte-cochere type structure over the window(s) for a length of twenty (20') feet shall be provided in an architectural style consistent with the main building style.
- b) The porte-cochere may encroach halfway into the side yard setback area when the adjacent use is neither single family nor multi-family residential use.

## **Sec. 11. Additional requirements, variances, adjustments, adaptations and exemptions**

### **Signs**

Signs shall be erected or installed according to the following criteria:

- a) *Wall Signs.* The maximum allowable wall sign area shall be one and one half (1.5) square feet per one (1) linear foot of building frontage. Total sign area shall be the sum of all sign areas excluding opening banners. No individual wall sign shall exceed one hundred (100) square feet in size for a building with less than two hundred (200) linear feet of building frontage. For buildings with building frontage exceeding two hundred (200) linear feet, no individual sign shall exceed two hundred (200) square feet in size.

Any neon lighting that is not an integral part of signage will be counted at a rate of .25 square feet of signage per lineal foot of neon lighting

- b) *Ground Signs.*
  1. Only one (1) ground sign shall be allowed per parcel with four hundred (400) feet or less road frontage. If a parcel's road frontage exceeds four hundred (400) feet and is less than seven hundred (700) feet then a maximum of two (2) ground signs shall be allowed, but no closer than three hundred (300) feet apart. If a parcel's road frontage exceeds seven hundred (700) feet, then a maximum of three (3) ground signs shall be allowed, but no closer than three hundred (300) feet apart.
  2. Vertical structural supports for ground signs shall be concealed in an enclosed base. The width of such enclosed base shall be equal to at least two-thirds (2/3) the horizontal width of the sign surface. A planter structure shall enclose the foot of the base. The planter shall be between two (2) and three (3) feet in height above the ground, with a minimum length equal to the width of the sign and a minimum width of three (3) feet. The base and planter shall be brick, masonry or other durable material compatible with the architecture and materials of the site.
  3. Any external above ground light source shall be located and hidden within the planter bed. Light sources located outside the planter bed shall be in a burial fixture.

4. The maximum height of the entire sign structure shall be fifteen (15) feet above the elevation of the nearest sidewalk.
  5. The planter setback shall be a minimum of five (5) feet from the right-of-way.
  6. The maximum allowable ground sign area shall be one and one half (1.5) square feet per one (1) linear foot of building frontage but shall not exceed one hundred (100) square feet. Ground sign base, sides and top are excluded from the sign area calculation.
- c) *Maximum total sign area.* In no case shall the sum of total ground sign square footage plus total wall sign square footage exceed two (2) square feet per one (1) linear foot of building frontage on any given parcel.
  - d) *Movement.* No ground sign nor its parts shall move, rotate or use flashing lights.
  - e) *Illumination.* Sign lights shall be focused, directed, and so arranged as to prevent glare or direct illumination or traffic hazard from said lights onto the abutting roadways. No flashing or pulsating lights shall be permitted on any sign.
  - f) *Prohibited signs.* Off-premise signs, portable signs, pole signs, and temporary signs
  - g) *Exempted signs.* Real estate signs.
  - h) *Opening banners.* Opening banners shall be allowed from two (2) weeks prior to opening until one (1) month after opening. Thereafter, banners are prohibited.
  - i) *Billboards.* New and replacement billboards are prohibited.
  - j) *Message Centers.* Electronic message centers may be allowed, but only as an integral part of the site's primary signage, and compromising no more than 50% of the total ground sign allocation. They may not create excessive glare or otherwise endanger passing motorists. Electronic message centers shall not display messages that give an illusion of motion and shall maintain each displayed message for a minimum of five (5) seconds.

#### Utility Lines and Structures

- a) *Lines.* All new or relocated utility lines within the designated corridor shall be constructed and installed beneath the surface of the ground unless it is determined otherwise by the board of county commissioners in exercising the public's proprietary rights over publicly owned rights-of-way.
- b) *Structures.* Utility structures such as lift stations, phone boxes and backflow preventers shall not be located in the front yard landscape buffer area.

#### Lighting

All site lighting shall be designed to minimize glare, light pollution, and light trespass while promoting high visual activity consistent with code requirements (see appendix).

#### Zoning variance criteria.

- 1) Landscape buffer width. The width may be reduced to a minimum of ten (10) feet when the lot is less than two hundred (200) feet deep. Tree and landscaping requirements still apply fully.

#### Design standard variances.

Variances to design standards set forth herein shall be determined in accordance with the procedures and standards set forth in Part 10, Chapter 40, for variance from site plan requirements. In addition, the Planning Manager is given discretion to modify the site design standards when such modification is consistent with the purpose and principles of this Part and the modification is the minimal modification necessary to enable infill and/or redevelopment to occur.

#### Exemptions

All residentially zoned single-family lots are exempted from meeting all standards contained in this Part except for the burial of utility service lines, wall requirements and the building setback requirement.

#### Sec. 12. Codification.

#### Sec. 13. Severability.

#### Sec. 14. Effective date.

